



Land East of Oxhey Lane, Carpenders Park

PINS Ref: APP/P1940/V/26/3378268

LPA Reference: 25/1020/OUT

Traffic and Transport Proof of Evidence of Philip Hamshaw  
BA (Hons) MSc MCIHT CMILT

Client: Burlington Developments London Ltd

i-Transport Ref: PH/ITL200107-012A R

Date: 02 June 2026

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## Quality Management

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## **SECTION 1 Introduction**

### **1.1 Qualifications and Experience**

1.1.1 My name is Philip Hamshaw. I am a Chartered Member of the Institute of Logistics and Transport and a Member of the Chartered Institution of Highways and Transportation. I have a Masters Degree in Transportation Planning & Engineering. I am a Partner of i-Transport LLP a specialist transport planning consultancy.

1.1.2 Before joining i-Transport LLP I was a Technical Director of SKM Colin Buchanan between 2008 and 2012, prior to which I was employed as a Director for Faber Maunsell. I have over 25 years' experience in the field of transport planning and engineering.

### **1.2 The Scheme**

1.2.1 The Scheme relates to an area to the east of the A4008 Oxhey Lane on the eastern side of Carpenders Park. The Scheme would comprise:

- Up to 256 new homes comprising a mix of affordable and market housing;
- Provision of Housing with Care (60 homes);
- Provision of a 4-bed children's home (Use Class C2);
- Three accesses onto A4008 Oxhey Lane:
  - Primary signal junction for all modes;
  - Pedestrian only access
  - Emergency Vehicle / Pedestrian / Cycle access; and
- Associated enhancements to transport infrastructure to support sustainable travel

### **1.3 Scope and Nature of Evidence**

1.3.1 The application was called-in by the Secretary of State on 25 March 2026. This immediately followed the Council's Planning Committee on 19 March 2026 when Members resolved to refuse permission contrary to a positive officer recommendation. The Call-In letter set out the key matters and these were confirmed at the Case Management Conference on 18 May 2026. The main issues are:

- Whether or not the application site is grey belt land and whether or not the proposal would be inappropriate development in the Green Belt, having regard to the National Planning Policy Framework and relevant Development Plan policies;
- The effect of the proposal on the openness and purposes of the Green Belt;
- The effect of the proposal on the character and appearance of the area, including on landscape;
- The effect of the proposed development on highway safety and on the efficient operation of the highway network; and,
- If inappropriate, whether any harm by reason of inappropriateness, and any other harm, would be clearly outweighed by other considerations, so as to amount to the 'very special circumstances' required to justify the proposal.

1.3.2 Accordingly, and with reference to NPPF paragraph 155 (c), I provide evidence on the following topics in my capacity as a professional expert on highway and transport matters namely:

- The safety and suitability of the access arrangements to the Site;
- The sustainable transport credentials of the Site location.; and
- The traffic and highway safety impact of the Scheme.

1.3.3 Overall, I conclude that:

- The combination of accesses provide safe and suitable access for all expected movements associated with the Scheme;
- The site is in a sustainable location and the Scheme will deliver improvements to active travel infrastructure and public transport services which will further enhance the sustainable transport credentials of the site location;
- The impact of the Scheme on highway safety would be acceptable and the traffic impact in terms of capacity would fall well short of 'severe'

1.3.4 There is no basis on highway or transportation grounds for refusing the Scheme.

## 1.4 **Background**

1.4.1 The planning application was accompanied by supporting documents assessing the transport impacts as follows:

- Transport Assessment (CD1.42)
- Framework Travel Plan (CD1.43)

1.4.2 Following the submission consultation comments were received from Hertfordshire County Council (HCC) Highways on 6 August 2025. Discussions were held with HCC Highways over an extended period (as set out in the Statement of Common Ground with HCC Highways CD6.7) which culminated in an agreement to provide a package of transport infrastructure and service improvements which were set out in Summary of Sustainable Transport Improvements (CD1.70).

1.4.3 I have prepared and agreed a Statement of Common Ground with HCC Highways (CD6.7). This confirms that all matters are agreed in respect of transport and highways. Specifically, it is agreed that:

- The access arrangements provide for safe and suitable access to the site for all users;
- The test of 'special circumstances' of LTP 4 Policy 5(f) would be satisfied if planning permission was granted;
- The public transport and local infrastructure improvements will maximise the sustainable transport solutions of the site location and encourage movement by sustainable transport modes; and
- Sustainable transport modes have been prioritised taking account of the vision for the site, the type of development and its location
- The impacts from the Scheme on the transport network are not significant in terms of capacity, congestion and highway safety and fall well below 'severe'

## 1.5 **Statement**

1.5.1 The evidence that I have prepared and provide for the Inquiry in this proof of evidence is true and has been prepared and is given in accordance with the guidance of my professional institutions and I can confirm that the opinions expressed are my true and professional opinions.

## SECTION 2 Relevant Policy and Guidance

### 2.1 National Policy

#### National Planning Policy Framework

2.1.1 The National Planning Policy Framework (NPPF) published in December 2024 sets out the Government's planning policies for England and how these are expected to be applied. It also constitutes guidance for local planning authorities and decision makers in drawing up plans and as material considerations in determining applications.

2.1.2 The specific transport policies are contained within Section 9 of the NPPF, and paragraph 115 identifies the following 'four factors' in relations to transport and highways matters:

***"In assessing sites that may be allocated for development in plans, or specific applications for development, it should be ensured that:***

***a) sustainable transport modes are prioritised taking account of the vision for the site, the type of development and its location;***

***b) safe and suitable access to the site can be achieved for all users;***

***c) the design of streets, parking areas, other transport elements and the content of associated standards reflects current national guidance, including the National Design Guide and the National Model Design Code; and***

***d) any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree through a vision-led approach***

2.1.3 In terms of the first factor (which I address further below) – sustainable modes are prioritised – it is clear that the NPPF does not advocate a one size fits all approach, i.e., what is appropriate in central urban locations will be different from what is appropriate in a suburban area or settlement. Importantly, the NPPF clearly does not seek to preclude or inhibit appropriate development in different location on sustainability grounds, as set out in Paragraph 110:

***"Significant development should be focused on locations which are or can be made sustainable, through limiting the need to travel and offering a genuine choice of transport modes. This can help to reduce congestion and emissions, and improve air quality and public health. However, opportunities to maximise sustainable transport solutions will vary between urban and rural areas, and this should be taken into account in both plan-making and decision-making."***

2.1.4 As to the second and fourth factors, I also address each of these in more detail in subsequent sections and explain how the development proposals deliver on the identified objectives. As this is an outline planning application with the design of streets, parking areas etc for reserved matters I do not directly address the third factor.

2.1.5 Paragraph 116 of the NPPF states that:

***Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network, following mitigation, would be severe, taking into account all reasonable future scenarios.***

2.1.6 There are no adverse impacts on highway safety in relation to the Scheme, but there would be benefits arising from the delivery of improvement to the pedestrian network with a new signal pedestrian crossing of the A4008 Oxhey Lane at the primary site access and improvements to the footways and crossings on Carpenders Avenue between the Site and local amenities. As to the road network, paragraph 116 of the NPPF sets a very high bar for preventing development from coming forward for transport reasons – it is only where there will be severe impacts (i.e., very significant consequences) that development should be refused on transport grounds. In fact there would not be an unacceptable impact on highway safety and there are no material adverse residual cumulative impacts on the road network from the development proposals, let alone any which could be classified as severe.

## 2.2 Local Policy

2.2.1 The Three Rivers District Council Core Strategy was adopted in 2011 and Development Management Policies LDD adopted in 2013. These documents provide the basis for planning decisions within Three Rivers.

### Three Rivers District Council – Core Strategy Policies

2.2.2 Policy CP1 – Overarching Policy on Sustainable Development states:

***“All development in Three Rivers will contribute to the sustainability of the District. This means taking into account the need to:***

***[...]***

***l) Reduce the need to travel by locating development in accessible locations and promoting a range of sustainable travel modes***

*m) Provide necessary infrastructure to enable and/ or support development, including (but not limited to) transport, education, health, green infrastructure, utilities, waste facilities, waste water, leisure, cultural and community facilities*

*[...]*

*Development will not be permitted unless it is demonstrated that sustainable development principles are satisfied through compliance with the policies of this Core Strategy and other relevant regional and national policy requirements."*

### 2.2.3 Policy CP10 – Transport and Travel states:

*"The Council will promote transport measures identified in the Infrastructure Delivery Plan in partnership with Hertfordshire County Council, the Highways Agency and transport providers.*

*Development proposals will be expected to contribute to the delivery of transport and travel measures identified as necessary for the development, either on-site as part of the development or through contributions to off-site provision as appropriate. Provision for interchange and access by public transport, walking and cycling will be regarded as particularly important.*

*Every opportunity will need to be taken to integrate means of travel. The following transport and travel measures will be supported at appropriate locations across the District, and development will need to consider the provision of measures and facilities that encourage integration including:*

- a) Secure cycle parking*
- b) A safe network for pedestrians*

*[...]*

#### *Development proposals*

*All development should be designed and located to minimise the impacts of travel by motor vehicle on the District. In particular, major development will be expected to be located in areas highly accessible by the most sustainable modes of transport, and to people of all abilities in a socially inclusive and safe manner, in accordance with the user hierarchy below. Priority will be given in the following order:*

- i. Pedestrians, particularly people with restricted mobility*
- ii. Cyclists and where appropriate, horse riders*
- iii. Public transport (including taxis)*
- iv. All forms of motor vehicles.*

*Development will need to demonstrate that:*

- i) It provides a safe and adequate means of access*

***j) It is appropriate in scale to the existing transport infrastructure, including public transport and, where necessary, infrastructure can be improved***

***k) It is integrated with the wider network of transport routes, including public rights of way and cycle paths where appropriate***

***l) It makes adequate provision for all users, including car and other vehicle parking, giving priority to people with mobility difficulties, pedestrians, cyclists and equestrians***

***m) It includes, where appropriate, provision for public transport either within the scheme or through contributions***

***n) The impact of the proposal on transport has been fully assessed; for major development this should be done through a comprehensive Transport Assessment detailing the measures that will be used to reduce impacts***

***o) The proposal is accompanied by a draft Green Travel Plan for prospective users and employees of the development for all major development.***

2.2.4 Policy CP1 and CP10 align broadly with the requirements of NPPF as set out above and which I deal with in Section 4. In respect of the specific requirements of CP10 the development proposals for the Site:

- Provide a safe and adequate means of access
- Are appropriate to the scale of existing transport infrastructure;
- Integrate with existing public rights of way (including appropriate improvements);
- Make adequate provision for all users with priority for pedestrians with three dedicated convenient accesses, then cyclists with two accesses (with a single vehicle access);
- Have been fully assessed through a Transport Assessment
- Include a Framework Travel Plan (with a full Travel Plan secured by Condition)

## 2.3 Hertfordshire County Council – Local Transport Plan 4

2.3.1 HCC Highways refers to several LTP 4 policies in their consultation response dated 6 August 2025. I have set out the relevant policies below along with some initial commentary and deal with further at Section 4.

2.3.2 Policy 1: Transport User Hierarchy states:

***“To support the creation of built environments that encourage greater and safer use of sustainable transport modes, the county council will in the design of any scheme and development of any transport strategy consider in the following order:***

***Opportunities to reduce travel demand and the need to travel***

***Vulnerable road user needs (such as pedestrians and cyclists)***

***Passenger transport user needs***

***Powered two wheeler (mopeds and motorbikes) user needs***

***Other motor vehicle user needs”***

2.3.3 The policy relates to transport schemes and strategies, although Policy 5 relates the priority of movement to development (see below).

2.3.4 Policy 2: Influencing Land Use Planning states:

***“The county council will encourage the location of new development in areas served by, or with the potential to be served by, high quality passenger transport facilities so they can form a real alternative to the car, and where key services can be accessed by walking and cycling.”***

2.3.5 This echoes the requirements of NPPF in respect of the opportunities for sustainable travel. I deal with this in Section 4 where I demonstrate there would be genuine opportunities for sustainable travel from the Site.

2.3.6 Policy 5: Development Management states:

***“The county council will to work with development promoters and the district and borough councils to:***

***a) Ensure the location and design of proposals reflect the LTP Transport User Hierarchy and encourage movement by sustainable transport modes and reduced travel demand.***

***b) Ensure access arrangements are safe, suitable for all people, built to an adequate standard and adhere to the county council’s Highway Design Standards.***

***c) Consider the adoption of access roads and internal road layouts where they comply with the appropriate adoption requirements and will offer demonstrable utility to the wider public. Where internal roads are not adopted the county council will expect suitable private management arrangements to be in place.***

***d) Secure developer mitigation measures to limit the impacts of development on the transport network, and resist development where the residual cumulative impact of development is considered to be severe.***

***e) Require a travel plan for developments according to the requirements of ‘Hertfordshire’s Travel Plan Guidance’.***

***f) Only consider new accesses onto primary and main distributor roads where special circumstances can be demonstrated in favour of the proposals.***

***g) Resist development that would either severely affect the rural or residential character of a road or other right of way, or which would severely affect safety on rural roads, local roads and rights of way especially for vulnerable road users. This should include other routes which are important for sustainable transport or leisure.”***

2.3.7 Taking each of these points in turn:

- a The proposals will encourage movement by sustainable modes (Section 4);
- b Safe and suitable access will be provided as I explain at Section 3;
- c The accesses and Internal roads will be constructed to adoptable standards;
- d The cumulative impact of the development will not be severe (Section 5);
- e A Framework Travel Plan has been prepared and a detailed Travel Plan will be secured by Condition;
- f The vehicle access arrangements have been accepted by HCC Highways. Specifically, it is agreed with the County that the proposed primary access accords with this policy (as explained in the Statement of Common Ground)
- g The Scheme will not severely affect safety on either rural roads or public rights of way. Indeed it would provide a notable improvement to the existing PROW across the Site as well delivering a safer and controlled pedestrian crossing of the A4008 Oxhey Lane to access the existing PROW.

2.3.8 The Scheme would accord with the LTP4 policies noted above.

## SECTION 3 Access Arrangements

3.1.1 The proposed access arrangements comprise:

- Primary signal junction with A4008 Oxhey Lane providing access for pedestrians, cyclists and vehicles;
- Dedicated pedestrian access onto Oxhey Lane north of the existing Care Home; and
- Emergency vehicle/pedestrian/cycle access onto Oxhey Lane.

3.1.2 The access locations are shown on the Movement and Access parameter plan showing how they would connect the development parcels within the site to existing transport network. All the access designs are agreed with HCC Highways as set out in the SoCG and it is agreed that the combination of accesses provide safe and suitable access for all.

### **Primary Access**

3.1.3 The primary access is shown on application drawing ITL200107-GA-002 (Appendix A) which shows a signal junction in the location of the existing A4008 Oxhey Lane/Carpenders Park Avenue junction. The Site access would provide the fourth arm to a new signal junction which would incorporate footways and a controlled pedestrian crossing of A4008 Oxhey Lane. The layout would provide a dedicated stage for the site access to enable all road users (including cyclists) to safely access and egress the Site.

3.1.4 The design accords with HCC's Design Guide. The design correctly followed HCC Highways Place and Movement Planning and Design Guide with reference to Manual for Streets 2 as was explained in the Access Option Appraisal report (Appendix A of the Transport Assessment).

3.1.5 HCC Highways agrees that the design is safe and suitable. The access design was subject to a Stage 1 Road Safety Audit which raised a number of matters which were dealt with in the Designers Response to the satisfaction of HCC Highways. HCC's Road Safety team undertook their own appraisal of the designs which confirmed subject to design changes at the detailed stage there were no safety concerns with the access designs as proposed.

3.1.6 As noted previously Policy 5 (f) of LTP4 identifies that HCC Highways will:

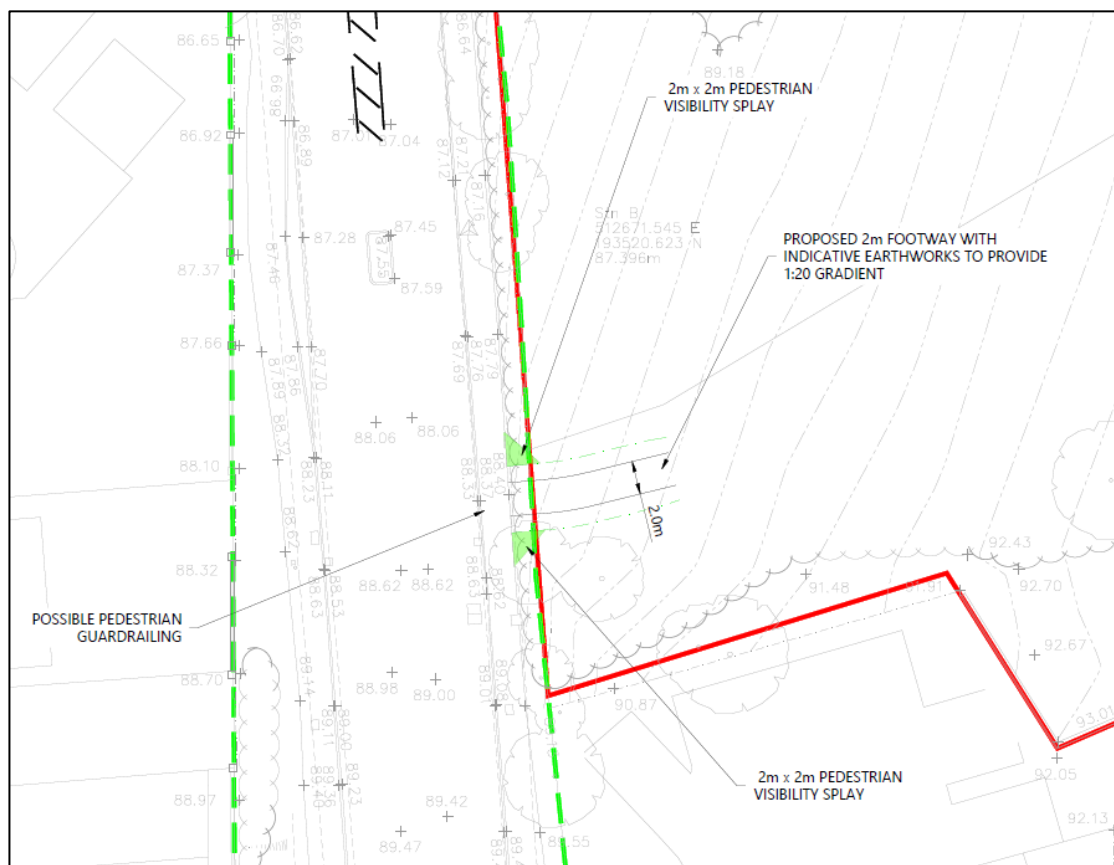
***Only consider new accesses onto primary and main distributor roads where special circumstances can be demonstrated in favour of the proposals***

- 3.1.7 Oxhey Lane is categorised by HCC as a Main Distributor Road under their road hierarchy. Thus, it is necessary to demonstrate special circumstances in support of a new access onto the A4008 Oxhey Lane.
- 3.1.8 As noted in the SoCG HCC Highways has confirmed that if development is acceptable in this location whether through Local Plan designation or the granting of planning permission that would constitute 'special circumstances' to permit an access onto a classified road. In such circumstances there would be no objection based on Policy 5(f).
- 3.1.9 It is noteworthy that HCC Highways did not object to the creation of a new access onto A4008 Oxhey Lane at Land to the Rear of Woodlands development to the north. The road category is the same.

**Pedestrian Access**

- 3.1.10 A dedicated pedestrian access is proposed immediately north of the existing Care Home and a design drawing (ITL200107-GA-008) of the pedestrian access has been prepared and is provided at **Appendix A** with an extract provided in Image 3.1 below.

**3.1.11 Image 3.1: Pedestrian access**



- 3.1.12 It is agreed with HCC Highways that subject to a future road safety audit the pedestrian access to Oxhey Lane (drawing ITL200107-GA-008) should provide for safe and suitable pedestrian access to the Site.

**Emergency Vehicle / Pedestrian / Cycle Access**

- 3.1.13 Emergency vehicle access from Oxhey Lane is proposed south of the existing Care Home. This will be 3.7m wide with a 1.0m buffer on both sides, with dropped kerb, corduroy paving, and a bollard structure to prevent unauthorised vehicular access but to ensure emergency access is achievable and pedestrian and cycle access is provided at all times. This is provided at drawing ITL200107-GA-007 Rev B at Appendix A.

**3.2 Conclusion**

- 3.2.1 Accordingly, I conclude the requirements of NPPF for safe and suitable access will be achieved for all users.

## SECTION 4 Sustainable Transport

4.1.1 This section of my evidence updates (where appropriate) the assessment in the TA in respect of sustainable transport and accessibility in the context of the following key transport test in National Planning Policy Framework paragraph 115:

***“In assessing sites that may be allocated for development in plans, or specific applications for development, it should be ensured that:***

***a) sustainable transport modes are prioritised taking account of the vision for the site, the type of development and its location;***

4.1.2 Alongside which I consider the following policies which provide comparable requirements to NPPF:

- CP1 – Overarching Policy on Sustainable Development
- CP10 – Transport and Travel
- LTP4 – Policy 1 and Policy 5

### 4.2 Pedestrians & Cyclists

4.2.1 Oxhey Lane routes north-south along the western Site frontage. Oxhey Lane has continuous footway provision on the eastern (Site) side of the carriageway and a continuous footway on the western side of the carriageway south of the junction with Carpenders Avenue. An uncontrolled pedestrian crossing on Oxhey Lane is situated 40m south of Carpenders Avenue and includes a refuge island, dropped kerbs and tactile paving.

4.2.2 There are currently no other pedestrian crossing facilities of Oxhey Lane in the vicinity of the Site.

4.2.3 Carpenders Park local centre and train station can be accessed from the Site via Carpenders Avenue. Footways are provided on both sides of the road, which are separated from the kerb by intermittent grassed verges and vehicle crossovers.

4.2.4 All crossing points along Carpenders Avenue have dropped kerbs but are not equipped with tactile paving. Street lighting is provided along Carpenders Avenue.

4.2.5 The network of pedestrian routes spreads throughout the residential area of Carpenders Park, providing pedestrian routes to primary schools, green space, local shops, Carpenders Park Station and through to South Oxhey.

4.2.6 Improvements to the pedestrian network will be provided, which are summarised as follows:

- Resurfacing and footway widening on Oxhey Lane south of the existing care home.
- Installation of a controlled signalised crossing (to be delivered as part of the site access works) at Oxhey Lane / Carpenders Avenue.
- Resurfacing of footway on south side of Carpenders Avenue up to the junction with Foxleys.
- Installation of tactile paving at:
  - Carpenders Avenue / Foxleys junction.
  - Foxleys / The Mead junction.
  - Delta Gain / Gibbs Couch junction.
  - Gibbs Couch near Carpenders Park Overground station.

4.2.7 These improvements (agreed with HCC Highways) would be secured by planning condition. The improvements align with the improvements to Carpenders Avenue identified in the LCWIP<sup>1</sup> and will deliver improved pedestrian accessibility.

4.2.8 The residential roads throughout Carpenders Park are relatively low trafficked and low speed. Therefore, in accordance with the MfS guidance cyclists can be safely accommodated on the carriageway.

4.2.9 The Site would have connections with the existing road network as I described in Section 3 and the internal layout would be designed in accordance with the principles of MfS to create a layout suitable for cycling on street. This would provide continuous routes through the development Site which connect into the existing roads on the surrounding network which link with facilities within Carpenders Park.

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<sup>1</sup> Three Rivers District Local Cycling and Walking Infrastructure Plan (LCWIP) adopted November 2025

4.2.10 Carpenders Avenue connects to the wider cycle network on Prestwick Road through an underpass under Carpenders Park Overground Station. An off-carriageway shared footway/cycleway is provided on the eastern side of the Prestwick Road and routes north to Bushey and Watford. This route is identified in HCCs LCWIP as a route suitable for walking and cycling. It is agreed with HCC Highways (SoCG – CD6.7) that this is a suitable route for cyclists between the site and Watford.

4.2.11 To further encourage cycle use a contribution of £20,000 towards the provision of three bikes at a Beryl Bike parklet in Carpenders Park and the provision of a dedicated Beryl Bike parklet on Site is proposed and agreed with TRDC Sustainable Transport Officer.

4.2.12 The improvements will improve accessibility by active travel modes.

### 4.3 Public Transport

#### London Overground

4.3.1 The nearest overground station is Carpenders Park located circa 1,200m (equivalent to a 15-minute walk<sup>2</sup> or a 4-5 minute cycle trip<sup>3</sup> although many can walk/cycle quicker) from the centre of the residential plots on Site. London Overground Lioness line serves the station with regular and reliable services every 15 minutes to destinations between London Euston and Watford Junction. A summary of the journey times to key destinations is provided in Table 4.1.

**Table 4.1: Local Rail Destinations**

Destination	Average Journey Duration
Euston	45 mins
Wembley Central	18 mins
Watford Junction	10 mins
Bushey	3 mins
Harrow & Wealdstone	9 mins
Willesden Junction	25 mins

Source: TfL

4.3.2 Services operate every day between approximately 05.30 and midnight.

<sup>2</sup> IHT Guidelines for Providing for Journeys on Foot (2000)

<sup>3</sup> On basis of 8km in 30 minutes (Sustrans Information Sheet FF11)

**Bus Services**

4.3.3 The closest existing bus stops 'Upper Hitch' (on By the Wood Road) is located 800m walking distance from the centre of the residential plots on Site. The bus stop infrastructure at the stops consists of a bus stop flag, timetable information and seating. Table 3.2 summarises the destination and frequency of the bus routes served by these bus stops. Table 3.2 also includes the 328 service which currently routes via Carpenders Park Overground station (on the South Oxhey side).

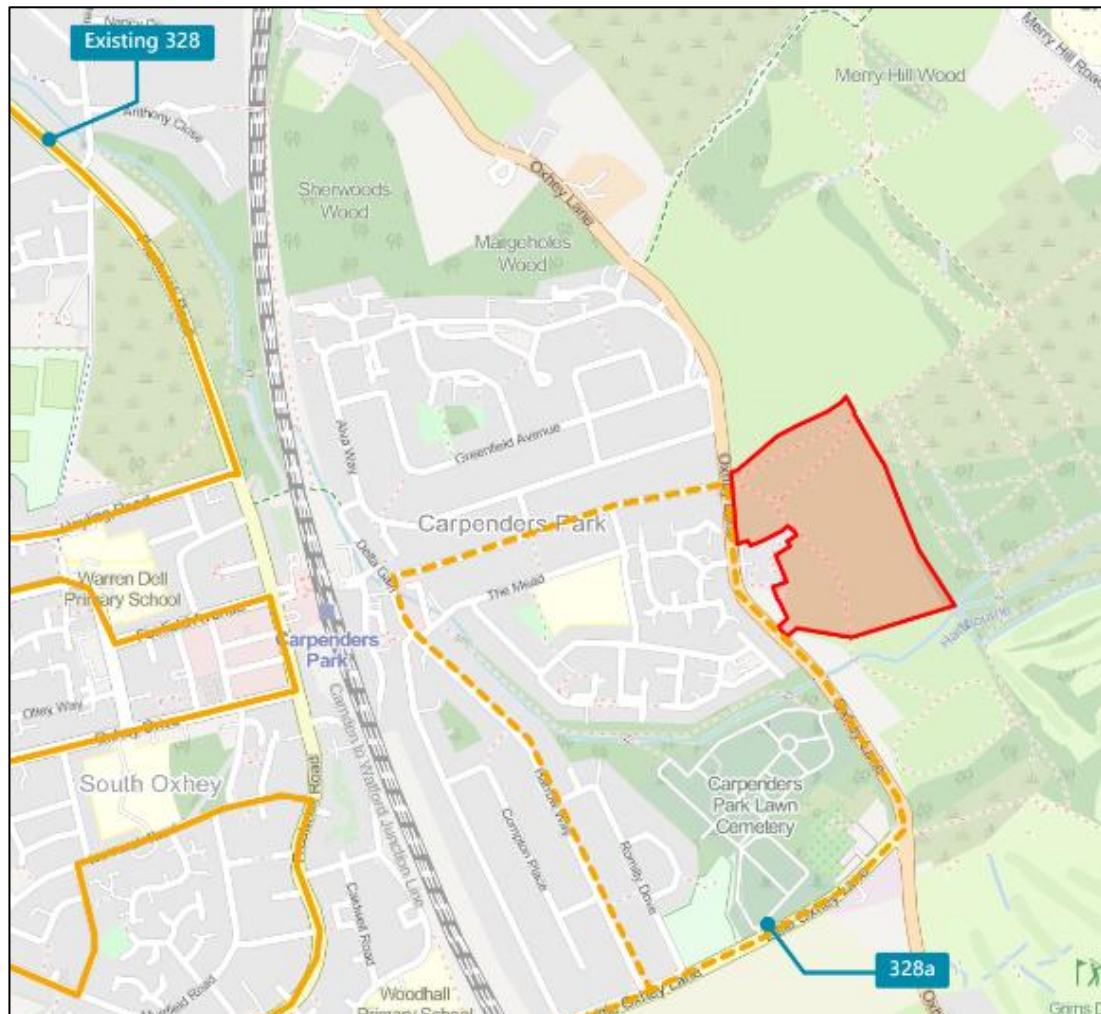
**Table 4.2: Local Bus Services**

Service No.	Route	Typical Frequency		
		Mon-Fri	Sat	Sun
346	Watford – Carpenders Park	Hourly 07:46 – 18:12	Hourly 08:14 – 18:14	-
R17	Carpender Park Railway – Hatch End Harrow Arts Centre	One bus on Wednesday at 10:14	-	-
R16	Bushey Railway Station – North Watford Sainsburys	Two buses on Monday and Friday only	-	-
328	Watford – Carpenders Park – Northwood	Hourly 05:55 – 20:05	Hourly 07:25 – 18:25	Hourly 09:40 – 17:40

Source: Bustimes.org (accessed May 2026)

4.3.4 The Scheme will deliver improvements to the 328 bus service which currently routes between Northwood and Watford via Carpenders Park Overground station (South Oxhey side). A bus service contribution identified by HCC public transport officers would provide funding for a bus to loop along Carpenders Avenue and Oxhey Lane alongside the Site as shown in Image 4.1 below.

**Image 4.1 – Route of proposed bus service**



4.3.5 This would be an extension/enhancement to the existing 328 service (*the 328a loop*). Along the loop it would provide an hourly service as follows:

- Monday to Saturday      First Bus: 05:53    Last Bus: 22:53
- Sunday                      First Bus: 08:53    Last Bus: 18:53

4.3.6 The existing 328 service is an established and viable service which would benefit from an increased frequency along its primary route as result of the *328a loop*.

4.3.7 A total contribution of **£613,558.69** over a five year period is agreed. The contribution has been calculated by HCC public transport officers based on forecast revenues and costs. It is agreed the contribution will support the operation of the bus service during a five year period as the Scheme proceeds enabling it to become self-sustaining beyond the funding period.

4.3.8 The level of contribution is staged over five years with years 4 and 5 having a lower contribution reflecting the expected patronage arising from the Scheme.

4.3.9 A new bus stop for the new bus service along Oxhey Lane will be located to ensure that the majority of the Scheme will be within a 400m walk distance (thus there is no need for the bus service to route into the Site). A financial contribution of £38,000 is agreed to secure the bus stop infrastructure including Shelter, Kassel Kerbing and RTI screen.

4.3.10 The contributions and new bus stop will improve the accessibility of the Site by public transport.

#### 4.4 **Travel Plan**

4.4.1 A detailed Travel Plan for the residential element and Care Home element of the Scheme will be secured by condition, with two support and evaluation payments of £6,000 secured by planning obligation. A Framework Travel Plan for the residential element of the Scheme was submitted with the application with the 10% target mode shift agreed as appropriate.

4.4.2 The measures included in the Framework Travel Plan are summarised below:

- Sustainable Travel Voucher for each household (£100)
- EV charging points for each dwelling with on plot parking
- Promotion of HCC Car Share scheme
- Travel Pack containing public transport travel information, maps and website links for each household
- Travel Plan Coordinator
- Information provision for walking, including route maps
- Information provision for cycling including route maps
- The Travel Plan will be managed and monitored to deliver against a target to reduce the number of private car trips to and from the site across the network peak hours by 10% from the baseline by Year 5. This target reflects the vision-led Transport Assessment.

4.4.3 The Travel Plans will encourage the use of sustainable modes of transport.

#### 4.5 **Opportunities for Sustainable Travel**

##### Walking Trends

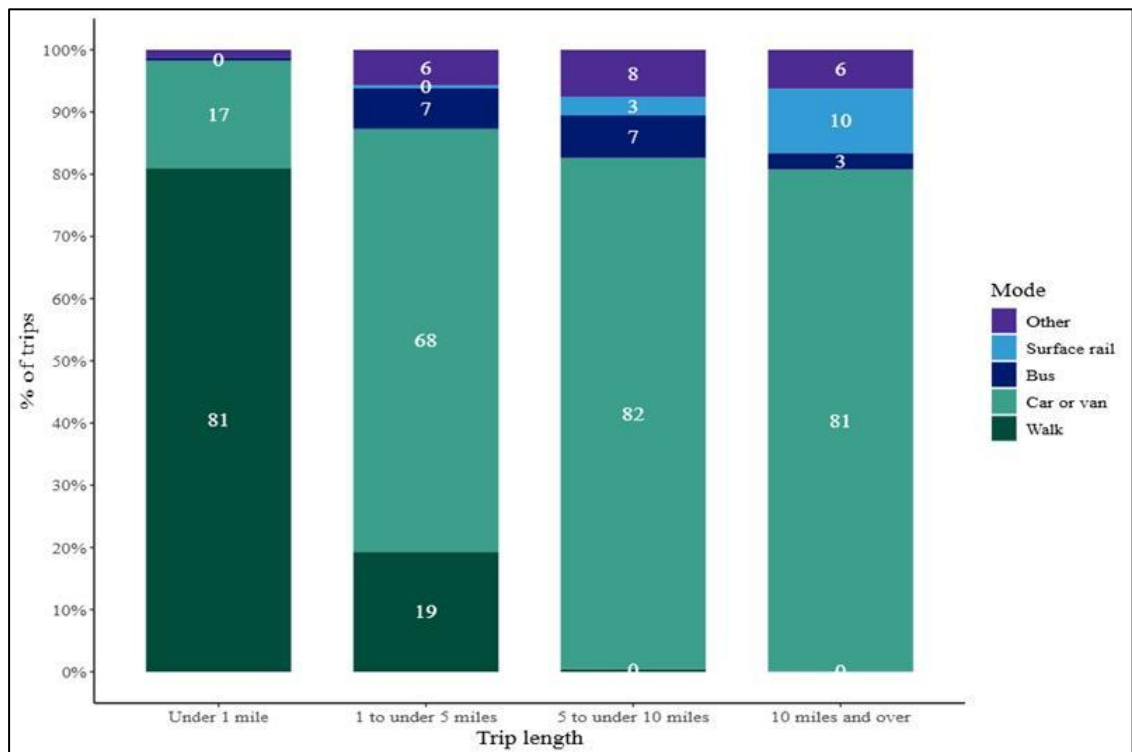
4.5.1 Paragraph 4.4.1 of the Manual for Streets identifies that:

***“Walkable neighbourhoods are typically characterised by having a range of facilities within 10 minutes’ (up to about 800 m) walking distance of residential areas” and “this is not an upper limit and PPG13 states that walking offers the greatest potential to replace short car trips, particularly those under 2km.”***

4.5.1 The data in the NTS (Table NTS308), identifies that the vast majority (81%) of trips of up to one mile (1.6km) are undertaken on foot. The data also shows that 19% of journeys between 1 and 2 miles will be on foot, i.e., a significant proportion of people are prepared to walk for journeys of up to 2 miles (3.2km). Walking is therefore a realistic and feasible option for many short trips.

4.5.2 **Image 4.2** from the National Travel Survey (2024) shows illustratively the change in mode share as the trip length increases.

**Image 4.2: Mode of Travel by Distance**



Source: National Travel Survey, England 2024

4.5.3 The one mile (1.6km) distance is reflected in Chartered Institution of Highways and Transportation (CIHT) guidance ‘Planning for Walking’ (2015) which states:

***“Across Britain, approximately 80% of journeys shorter than 1 mile are made wholly on foot – something that has changed little in 30 years. The main reason for the decline in walking is the fall in the total number of journeys shorter than 1 mile, which has halved in thirty years. It is not that people are less likely to make short journeys on foot but rather that fewer of the journeys they make can be accomplished on foot. If destinations are within walking distance, people are more likely to walk if walking is safe and comfortable and the environment is attractive.”***

- 4.5.4 Therefore, providing new homes within one mile of facilities and services would provide the greatest opportunity for trips to be made on foot.
- 4.5.5 That is not to say that a mile is the maximum that people are prepared to walk, or that development must be located within a mile of everything. It is clear from the NTS data that around one-third of journeys between one and two miles are undertaken on foot.
- 4.5.6 Against all of this background, the following walking distances are identified:
- 800m – i.e., under a 10-minute walk typically characterised as a “walkable neighbourhood”;
  - 1,600m or one mile – i.e., the distance within which circa three-quarters of journeys are made on foot;
  - 2,000m – i.e., the distance that “offers the greatest potential to replace short car trips”; and
  - 3,200m or two miles – i.e., the distance within which a significant proportion (circa one-third) of journeys will be on foot.

### Cycling Trends

- 4.5.7 The Department for Transport’s Cycling and Walking Investment Strategy (2017) states at paragraph 1.16 that:

***“... there is significant potential for change in travel behaviour. Two out of every three personal trips are within five miles - an achievable distance to cycle for most people, with many shorter journeys also suitable for walking. For school children, the opportunities are even greater. Three quarters of children live within a 15 minute cycle ride of a secondary school, while more than 90% live within a 15 minute walk or bus journey from a primary school.”***

- 4.5.8 The DfT’s Gear Change A bold vision for cycling and walking states (page 11) that:

***“In particular, there are many shorter journeys that could be shifted from cars, to walking, or cycling. We want to see a future where half of all journeys in towns and cities are cycled or walked. 58% of car journeys in 2018 were under 5 miles. And in urban areas, more than 40% of journeys were under 2 miles in 2017–1817. For many people, these journeys are perfectly suited to cycling and walking.”***

- 4.5.9 Paragraph 2.2.2 of the DfT Document LTN 01/20 ‘Cycle Infrastructure Design’ discusses typical cycle trip distances and states that ***“Two out of every three personal trips are less than five miles (8km) in length –an achievable distance to cycle for most people, with many shorter journeys also suitable for walking”***.

4.5.10 A cycling distance of over 2km offers an active travel mode alternative to walking and provides the opportunity to replace short car trips. A cycling distance of up to around 5km (3 miles) therefore offers the greatest potential to replace cars trips and is therefore a “reasonable” cycling distance, although a number of cycle journeys may be longer at 8km (5 miles). Cycling also frequently forms part of a longer journey in combination with public transport.

#### **Accessibility to Local Facilities**

4.5.11 Table 4.3 shows that there are a range of leisure, shopping, healthcare and educational services and facilities within Carpenders Park.

**Table 4.3 Site Accessibility Review**

Purpose	Destination	Distance (m)	Walking Journey Time (mins)	Cycle Journey Time (mins)
Leisure	Roots Hairdresser	800	10	3
	Absolute health & beauty	800	10	3
	Pizza Delight	800	10	3
	The Partridge Pub	950	12	3
	Carpenders Park Community Hall	1,000	11	4
	Carpenders Park Garden Centre	1,150	16	5
	Green Park Play Area	1,250	17	5
	Carpenders Park Skate & BMX Park	1,350	19	6
	Oxhey Library	1,350	18	7
	South Oxhey Leisure Centre	1,550	21	7
	Grims Dyke Golf Club	1,750	25	7
	South Oxhey Choir	1,850	25	8
	South Oxhey Family Centre	1,850	25	8
Education	Little hearts pre-school	550	7	2
	St. Meryl Primary School	550	7	2
	Warren Dell Primary School	1,650	22	7
	Colnbrook School	1,650	22	7
	Oxhey Wood Primary School	1,850	25	8
	St. Joseph's Primary School	2,050	28	9
Retail	Carpenders Park Sub Post Office	800	10	3
	L Cook Florist	800	10	3
	Co-op Food	800	10	3
	Nisa Local	1,050	16	6
	Tesco Express	1,150	16	6
	Lidl	1,250	18	7
Healthcare	DB Jones Pharmacy	800	10	3
	Viks Pharmacy	1,100	15	5
	Esom's Pharmacy	1,250	17	6
	Manor View Practice	1,650	22	7

Purpose	Destination	Distance (m)	Walking Journey Time (mins)	Cycle Journey Time (mins)
	Dale Pharmacy	1,750	24	7

Source: Google Maps and Consultants estimates.

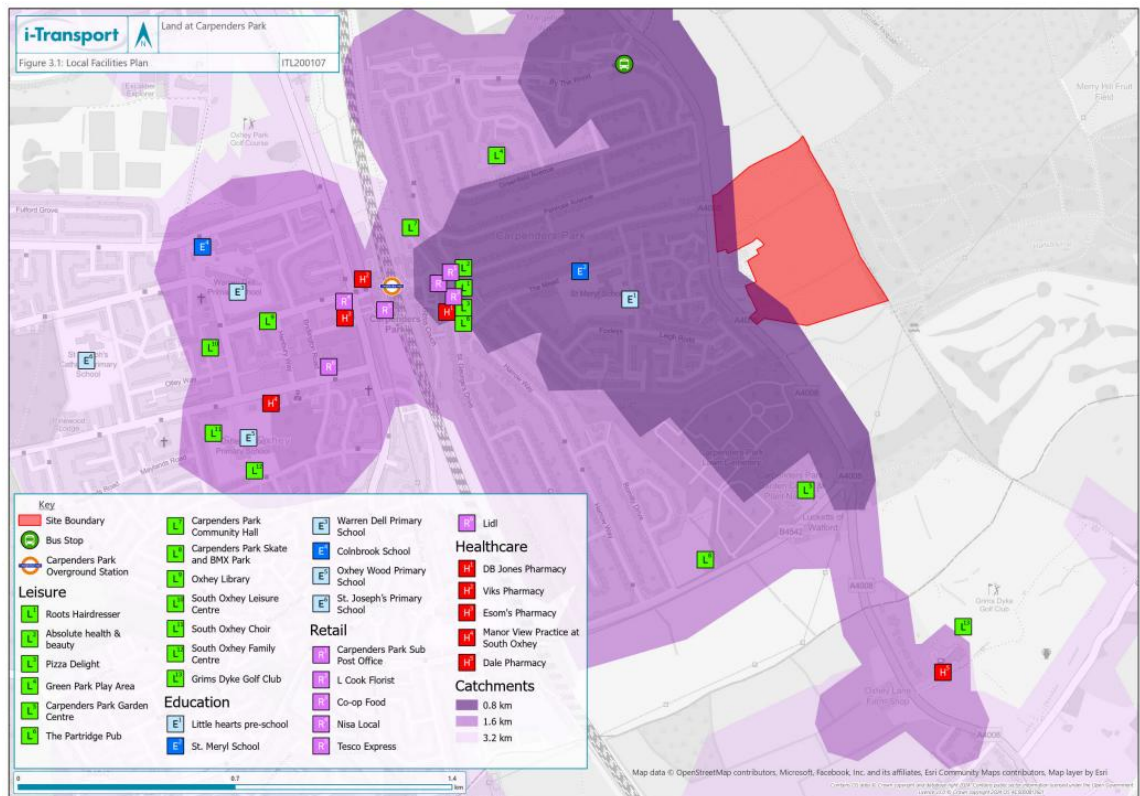
Walk times using the nearest access point from centre of site.

Key:

- Within a 'Walkable Neighbourhood' (800m)
- Within a distance where most people (circa 80%) will walk (1,600m)
- Within a distance where walking is a realistic alternative to car use and where some people (circa 31%) are still prepared to walk (3,200m)

4.5.12 The walk distances from the Site along with the location of the available local facilities are shown graphically on Figure 4.1 attached, with an extract below at Image 4.3.

**Image 4.3 Walking Isochrone and Local Facilities**

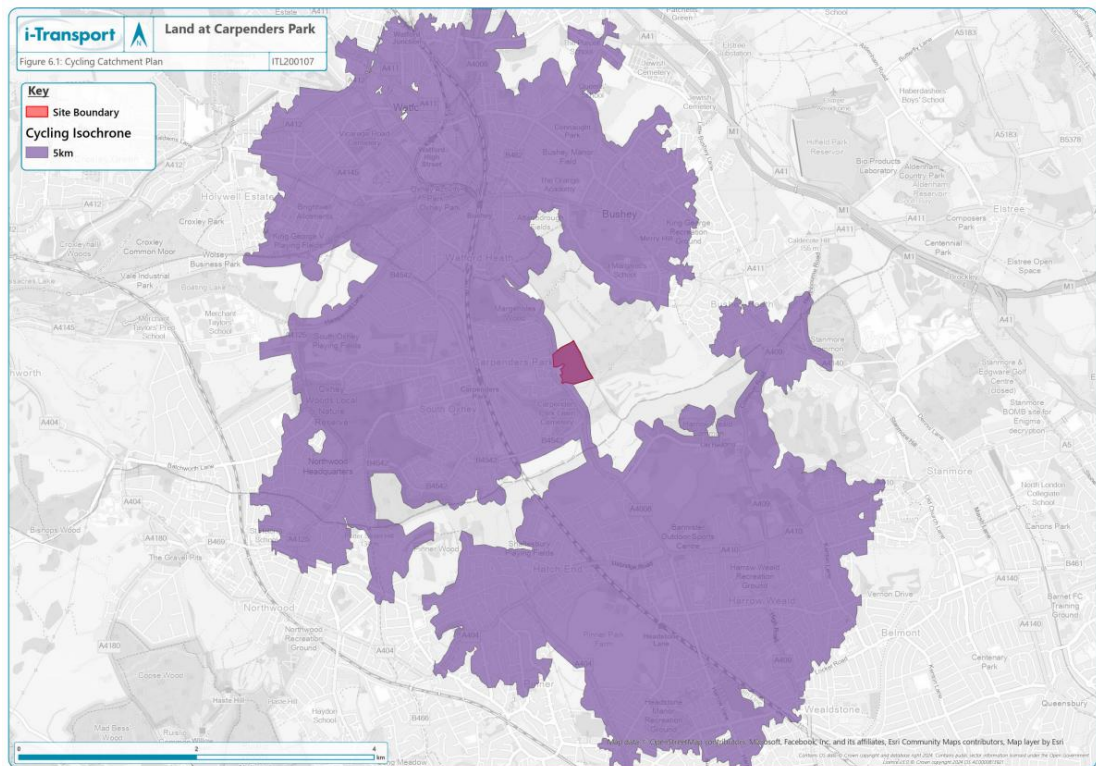


- 4.5.13 As the table and figures demonstrate many of the current services and facilities in Carpenders Park would be within 800m (less than half a mile), which is a comfortable walking distance and many more would be within 1,600 metres, a reasonable walking distance. Therefore, future residents would have the opportunity to walk to these services and facilities, which could cater for a large proportion of journey purposes.
- 4.5.14 Against this context the National Travel Survey (2024<sup>4</sup>) shows that around 64% of all trips are to leisure, shopping and educational services and facilities. Therefore, up to 64% of trips for various journey purposes could be undertaken within Carpenders Park by walking. For example, St Meryl Primary School and the Co-Op are located in well within a comfortable walking distance of the Site.
- 4.5.15 All of the facilities within Carpenders Park and South Oxhey are within cycling distance of the Site. The cycle catchment from the Site is shown graphically on Figure 4.2 with an extract below.

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<sup>4</sup> National Travel Survey 2024 (August 2025) NTS0409: Average number of trips and distance travelled by purpose and main mode: England, 2002 onwards.

Image 4.4 Cycling Isochrone



4.5.16 It can be seen that the 5km cycling catchment extends to Watford providing the opportunity to travel by bike to the further facilities and employment opportunities available there.

4.5.17 Notably, Carpenders Park Overground Station is within reasonable walking distance of the Site providing the ability to access destinations such as Bushey, Watford, Harrow, Wembley and central London. The Site is also well within cycling distance of Carpenders Park Overground station with existing cycle parking provision located on the South Oxhey side of the station. Added to which, as noted previously, a contribution will be provided to locate 3 new Beryl Bikes at parklet close to the station. The higher order facilities available in these larger urban areas provide further options for shopping and leisure trips (45%) as well as commuting and business trips (18% of all trips)<sup>1</sup>.

**Table 4.4: Workplace destinations accessible by public transport**

Destination	Employment Trips	Accessible via Bus	Accessible via Overground
Watford	24.9%	✓	✓
Harrow / Stanmore / Pinner	13.0%	-	✓
Rickmansworth	8.2%	-	-
Ruislip / Uxbridge	7.5%	-	-
Carpenters Park / South Oxhey	7.1%	✓	-
Wembley / Neasden	6.7%	-	✓
Other – London	6.2%	-	✓
Hendon / Edgware	4.3%	✓	-
Other – East	3.8%	-	-
Hemel Hempstead	3.3%	-	-
Northolt	3.3%	-	-
Other – North	3.0%	-	✓
Other – South	2.9%	-	✓
St Albans	2.8%	-	-
Borehamwood	2.1%	-	-
Other - West	0.9%	-	-
<b>Total</b>	<b>100.0%</b>	<b>36.3%</b>	<b>56.7%</b>

Source: Consultant

4.5.18 Table 4.4 shows that of the likely workplace destinations for existing and future residents of Carpenters Park, 56.7% of destinations are accessible using London Overground services, 36.3% of destinations are accessible using existing or proposed bus services. There is genuine opportunity for a large proportion of residents to travel to workplace destination by public transport.

## 4.6 DfT Connectivity Tool

4.6.1 The Department for Transport (DfT) Connectivity Tool provides a national connectivity metric. It shows how well places are connected to everyday destinations by walking, cycling and public transport. It is a useful starting point for establishing the existing connectivity (and thus sustainable transport credentials) of a location.

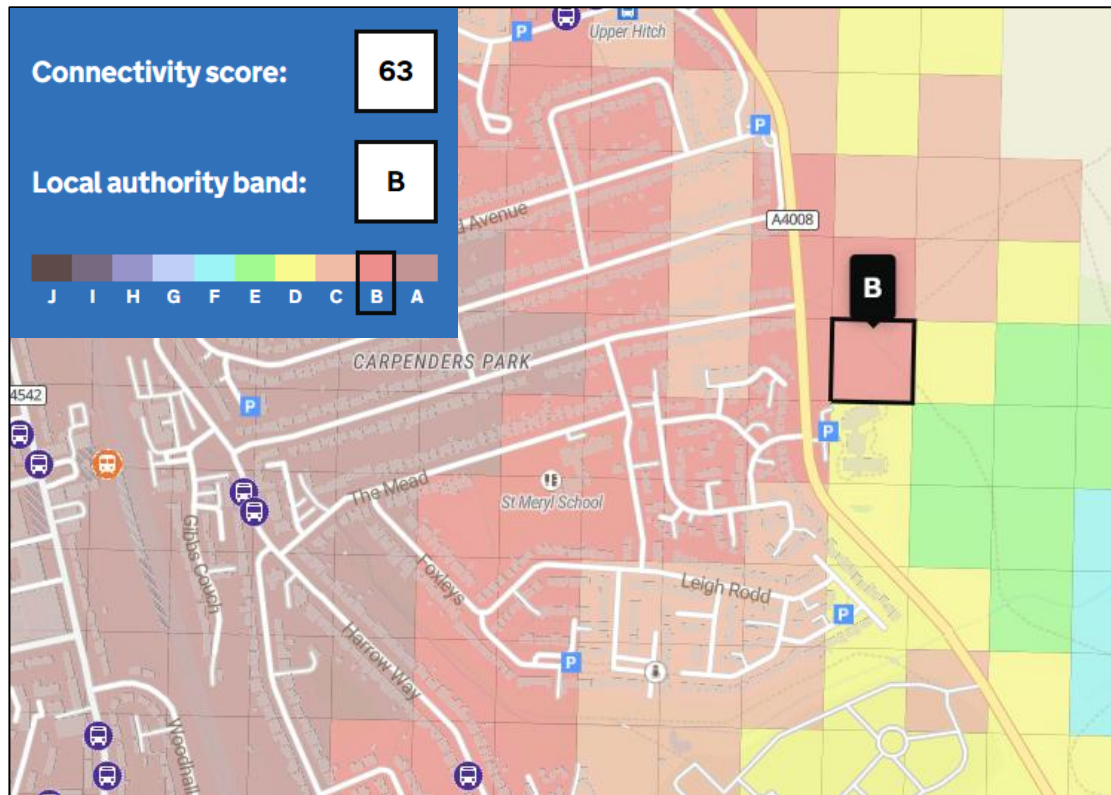
4.6.2 An extract from the Connectivity Tool map for the Carpenters Park location is shown in Image 4.5 below.

Image 4.5 Extract from Connectivity Tool



4.6.3 It can be seen the connectivity metric is 63 close to Oxhey Lane (although this reduces to 53 further from Oxhey Lane). This is on a national scale. The DfT note that ***It's critical to consider the wider context of locations when interpreting the scores.*** Thus, a further measure provided by the Connectivity Tool is at a Local Planning Authority level. In other words assessing the site location across a district rather than nationally. At LPA level the tool gives a banding ranging from A to J, with A being the most connected. It can be seen from Image 4.6 below that the Site currently sits within the range B to E, which lies in the 50<sup>th</sup> to 90<sup>th</sup> percentile for Three Rivers district.

**Image 4.6 Extract from Connectivity Tool**



4.6.4 The score does not account for any improvements to public transport proposed as part of the Scheme particularly the creation of a new bus stop serving a new service alongside the Site. Also, it does not account for the quality of pedestrian routes which will be improved. Nonetheless, the Site currently performs well at a district level confirming it is a sustainable location.

**4.7 Kings Farm Thurrock (APP/M1595/W/25/3358576)**

4.7.1 The Rule 6 party Residents Protecting Oxhey Lane Fields in their Statement of Case refer to the Kings Farm decision of 2026 for 690-750 homes in the Green Belt (APP/M1595/W/25/3358576) which was found to be Grey Belt, but found not to meet NPPF paragraph 155(c) criteria on sustainable location.

4.7.2 I have reviewed the decision and compared the sustainable transport credentials of each scheme in Table 4.5 below. I am aware that the Inspector present at this appeal was also the Inspector in the Kings Farm case and will therefore have his own familiarity with the circumstances of that site and proposed development.

**Table 4.5: Comparison between Site and Kings Farm, Thurrock**

Sustainability indicator	Kings Farm, Thurrock	Land East of Oxhey Lane
Proximity to existing local facilities	Primary school – 1.9km Secondary school – 4.0km Health centre – 4.0km Local shop – 3.0km Village hall – 1.4km Green space / park – 1.4km	Primary school – 550m Secondary school – 3.2km Health centre – 1.6km Local shop – 800m Village hall – 1.0km Green space / park – 1.1km
Active Travel Access	One pedestrian One cycle access Footways at site access	Three Pedestrian accesses Two Cyclist Accesses Dedicated pedestrian crossing
Active travel Improvements	New segregated footway / cycleway to village centre New footway to village centre	Signalised pedestrian crossing of Oxhey Lane Improvements to existing footways on Oxhey Lane Improvements to existing footways on Carpenders Avenue Beryl Bike Parklet contribution and Beryl Bike Parklet within the site
Rail	West Horndon station 5.6km from site 4tph to London (27 min) and Shoeburyness	Carpenders Park Overground station 1.2km from the site. 4tph to London (45min) and Watford (10min).

Sustainability indicator	Kings Farm, Thurrock	Land East of Oxhey Lane
Existing Bus Service	Nearest existing bus stops 1.2km from the site  One bus service, hourly	Nearest existing bus stops 800m and 950m from the site  Two bus services, hourly
Proposed Bus improvements	Rerouting of bus into site  Scheme to subsidise bus service up to 11 years from occupation  Hourly service. Late start time and early finish times  Bus routes not connect to major employment areas to the south  Possible £250k towards school bus service (no agreement with operator)	Extension / enhancement to existing 328 service (328a).  Hourly service throughout day, evening and weekends  328 is established and viable service  Funding over a five year period  Service self sustaining beyond funding period agreed with HCC public transport officers

4.7.3 It can be seen that the Kings Farm site performs poorly in terms of connectivity and sustainable transport measures when compared to the Scheme on the Site. Indeed with reference to paragraph 155 (c) there is a clear difference between the two.

## 4.8 Conclusion

4.8.1 The Site is in a sustainable location with a genuine choice of transport modes providing access to a range of local facilities within walking and cycling distance and well connected to the public transport network notably Carpenters Park Overground Station.

4.8.2 The sustainable transport credentials of the Site location will be further enhanced through the agreed improvements to active travel facilities and public transport services and infrastructure.

4.8.3 Thus, in respect of transport the Scheme is in a sustainable location (as per paragraph 155(c) of NPPF) and complies with:

- Paragraph 110 of the NPPF with there being a genuine choice of transport modes; and

- Paragraph 115 with sustainable modes being prioritised in accordance with the vision for the site having regard to the type and location of development.

## **SECTION 5    Traffic Impact**

- 5.1    The traffic modelling methodology and results provided within the Transport Assessment and subsequent technical notes have been audited and reviewed by the Local Highway Authority. As set out in the SoCG it is agreed that the methodology used to assess the impacts of the Scheme's related traffic followed best practice and used the appropriate traffic modelling software.
- 5.2    The modelling results are considered acceptable by the Local Highway Authority. In particular, it is agreed in the SoCG that the introduction of a signalised junction will not result in unacceptable delay to traffic flow on the A4008 Oxhey Lane.
- 5.3    Thus, as agreed within the SoCG the impacts from the Scheme on the transport network are not significant in terms of capacity, congestion and highway safety and fall well below 'severe'.
- 5.4    The London Borough of Harrow (LBH) raise concerns with respect to traffic impact and I deal with these in the following Section.

## SECTION 6 London Borough of Harrow

6.1 LBH object to the Scheme on a number of grounds. With respect to highways, LBH summarise their objection in their planning consultation response to the Call-In dated 11 May 2026 as follows:

**Highways:**

**•Potential for overspill parking**

**•Lack of information showing how the development will impact on the junction of Oxley Lane and Uxbridge Road (sited within Harrow boundaries).**

**•The proposed CMP/CEMP should consider impacts on the wider surrounding highway network, including Harrow. We request that no construction routes are on Harrow roads, with the majority of the routes keeping to the strategic highway network.**

**•Lack of detail re: access arrangements and traffic modelling for the Harrow strategic highway network, meaning a robust assessment of the development's impact on the Harrow's highway network cannot be made.**

6.2 This can be compared with their original consultation response which raised concerns relating to:

- Overspill parking
- Impact on Oxhey Lane/Uxbridge Road junction;
- Suitability of access arrangements; and
- Construction traffic management and routing.

6.3 The recommendation of the LBH highways officer was:

***Without detail relating to the access arrangements and traffic modeling for the Harrow strategic highway network, a recommendation for approval cannot be supported as no robust assessment of the developments impact on the Harrow highway network can be made.***

6.4 I deal with each of these points in turn.

### Overspill Parking

6.5 LBH concern relates to the impact on their highway network of any parking which is not contained within the Site. Whilst this is an outline planning application and parking provision would be a reserved matter, I consider the concern is unfounded for the following reasons:

- The closest road within LBH is 1.4km south of the Site. In the unlikely event that there will be any overspill parking from the Scheme, there is no realistic prospect that any vehicles will park on the Harrow highway network; and
- Car parking will be provided in line with TRDC parking standards, including visitor parking on-site. Overspill parking is therefore not anticipated.

### **Impact on Oxhey Lane/Uxbridge Road junction**

6.6 LBH concern relates to the traffic impact on the operation of this junction. I note that in their original consultation response LBH states:

***The impact of the development's trip generation on Harrow's highway network will be minimal, given the proximity of the site from the boundary, and the barrier the strategic highway network will provide to the dissipation of trips into Harrow.*** (my emphasis)

6.7 I agree. The impact will be minimal on LBH's highway network.

6.8 Some traffic will inevitably route through the Oxhey Lane/Uxbridge Road junction, based on the expected distribution of traffic as contained in the TA (which is not disputed by LBH) there would at peak hours be between 40 and 50 vehicle movements through the junction. Such increases are low equating to less than one additional vehicle per minute.

6.9 Larger increases in traffic flow are expected at the Oxhey Lane/Little Oxhey Lane junction located between the Site and the Oxhey Lane/Uxbridge Road junction. HCC HIGHWAYS accepted that no assessments were warranted at this junction.

6.10 Based on my own on site observations, the junction operates well with no noticeable delay or queuing. This is confirmed with reference to the LBH Transport Local Implementation Plan that identifies the most congested roads in the borough. Neither Oxhey Lane nor Uxbridge Road are included in that list.

6.11 I therefore conclude that there will be no impact on the continued satisfactory operation of the Oxhey Lane/Uxbridge Road junction as a result of the Scheme.

### **Suitability of access arrangements**

6.12 LBH consider the current access arrangement and detail is not suitable for the scale of development.

6.13 LBH is not the highway authority for the A4008 Oxhey Lane at the Site access location. HCC is the highway authority. HCC HIGHWAYS has confirmed that the access arrangements are safe and suitable.

### **Construction traffic management and routing**

- 6.14 LBH note that the Construction Management Plan should consider the impacts on the wider road network. The CMP will be secured by planning condition. The scope of the document will be agreed with HCC who may wish to consult LBH. It would necessarily consider all routes to the strategic road network that would be used during the construction of the Scheme.
- 6.15 LBH request that no construction routes use their road network. This is an unreasonable and unsubstantiated request. The A4008 Oxhey Lane forms part of the A-road network which continues through LBH. Similarly Uxbridge Road (A410) is an A-road. Both are suitable to accommodate construction traffic forming part of the primary road network.
- 6.16 The CMP would identify appropriate routes for construction vehicles as well as control the use of those routes. In addition it would control the type of construction vehicles and the timing of deliveries.

### **Conclusion**

- 6.17 I conclude that there is no basis for the highways objections raised by LBH.

## SECTION 7 Summary & Conclusions

### 7.1 Summary

7.1.1 The Scheme relates to an area to the east of the A4008 Oxhey Lane on the eastern side of Carpenders Park. The Scheme would comprise:

- Up to 256 new homes comprising a mix of affordable and market housing;
- Provision of Housing with Care (60 homes);
- Provision of a 4-bed children's home (Use Class C2);
- Three accesses onto A4008 Oxhey Lane:
- Primary signal junction for all modes;
- Pedestrian only access
- Emergency Vehicle / Pedestrian / Cycle access; and
- Associated enhancements to transport infrastructure to support sustainable travel

7.1.2 The proposed access arrangements comprise:

- Primary signal junction with A4008 Oxhey Lane providing access for pedestrians, cyclists and vehicles;
- Dedicated pedestrian access onto Oxhey Lane north of the existing Care Home; and
- Emergency vehicle/pedestrian/cycle access onto Oxhey Lane.

7.1.3 The access locations are shown on the Movement and Access parameter plan showing how they would connect the development parcels within the site to existing transport network. All the access designs are agreed with HCC Highways as set out in the SoCG and it is agreed that the combination of accesses provide safe and suitable access for all.

7.1.4 Further, as noted in the SoCG HCC Highways has confirmed that if development is acceptable in this location whether through Local Plan designation or the granting of planning permission that would constitute 'special circumstances' to permit an access onto a classified road. In such circumstances there would be no objection based on Policy 5(f).

7.1.5 The Site is in a sustainable location and the Scheme would materially improve its sustainable transport credentials. Future residents would have access to a wide range of day-to-day services

and facilities within walking and cycling distance, together with convenient access to Carpenters Park Overground Station and local bus services.

7.1.6 The agreed package of mitigation and enhancement measures includes:

- Resurfacing and footway widening on Oxhey Lane south of the existing care home.
- Installation of a controlled signalised crossing (to be delivered as part of the site access works) at Oxhey Lane / Carpenters Avenue.
- Resurfacing of footway on south side of Carpenters Avenue up to the junction with Foxleys.
- Installation of tactile paving at:
  - Carpenters Avenue / Foxleys junction.
  - Foxleys / The Mead junction.
  - Delta Gain / Gibbs Couch junction.
  - Gibbs Couch near Carpenters Park Overground station.
- Bus Service Contribution - **£613,558.69** has been agreed to support the operation of a bus service during a five year period as the development proceeds enabling it to become self-sustaining beyond the funding period.
- New bus stop - for the new bus service along Oxhey Lane with a financial contribution of £38,000 is agreed to secure the bus stop infrastructure including Shelter, Kassel Kerbing and RTI screen.
- Beryl Bike Contribution - £20,000 towards the provision of three bikes at a Beryl Bike parklet in Carpenters Park and the provision of a dedicated Beryl Bike parklet on site.
- Travel Plan - A Framework Travel Plan for the residential element of the scheme was submitted with the application with the 10% target mode shift agreed as appropriate.

7.1.7 The impacts of the Scheme on highway safety, network operation and capacity would be acceptable. HCC Highways has reviewed the assessment work and agreed the methodology, results and conclusions.

7.2 I have also considered the following concerns raised by the London Borough of Harrow:

- Overspill parking

- Impact on Oxhey Lane/Uxbridge Road junction;
- Suitability of access arrangements; and
- Construction traffic management and routing.

7.3 In response I set out that:

- Overspill parking will not occur and will not affect LBH road network;
- There will be no material impact on Oxhey Lane/Uxbridge Road junction;
- The suitability of access is not a matter for LBH; and
- Construction traffic will be controlled through an appropriate management plan.

## 7.4 Conclusion

7.4.1 Overall I conclude that:

- The requirements of NPPF for safe and suitable access will be achieved for all users.
- The Site is in a sustainable location with a genuine choice of transport modes providing access to a range of local facilities within walking and cycling distance and well connected to the public transport network notably Carpenders Park Overground Station.
- The sustainable transport credentials of the Site location will be further enhanced through the agreed improvements to active travel facilities and public transport services and infrastructure.
- Thus, in respect of transport the Scheme is in a sustainable location (as per paragraph 155(c) of NPPF) and complies with:
  - Paragraph 110 of the NPPF with there being a genuine choice of transport modes; and
  - Paragraph 115 with sustainable modes being prioritised in accordance with the vision for the site having regard to the type and location of development.
- The impacts from the Scheme on the transport network are not significant in terms of capacity, congestion and highway safety and fall well below 'severe'; and
- There is no basis for the highways objections raised by LBH.

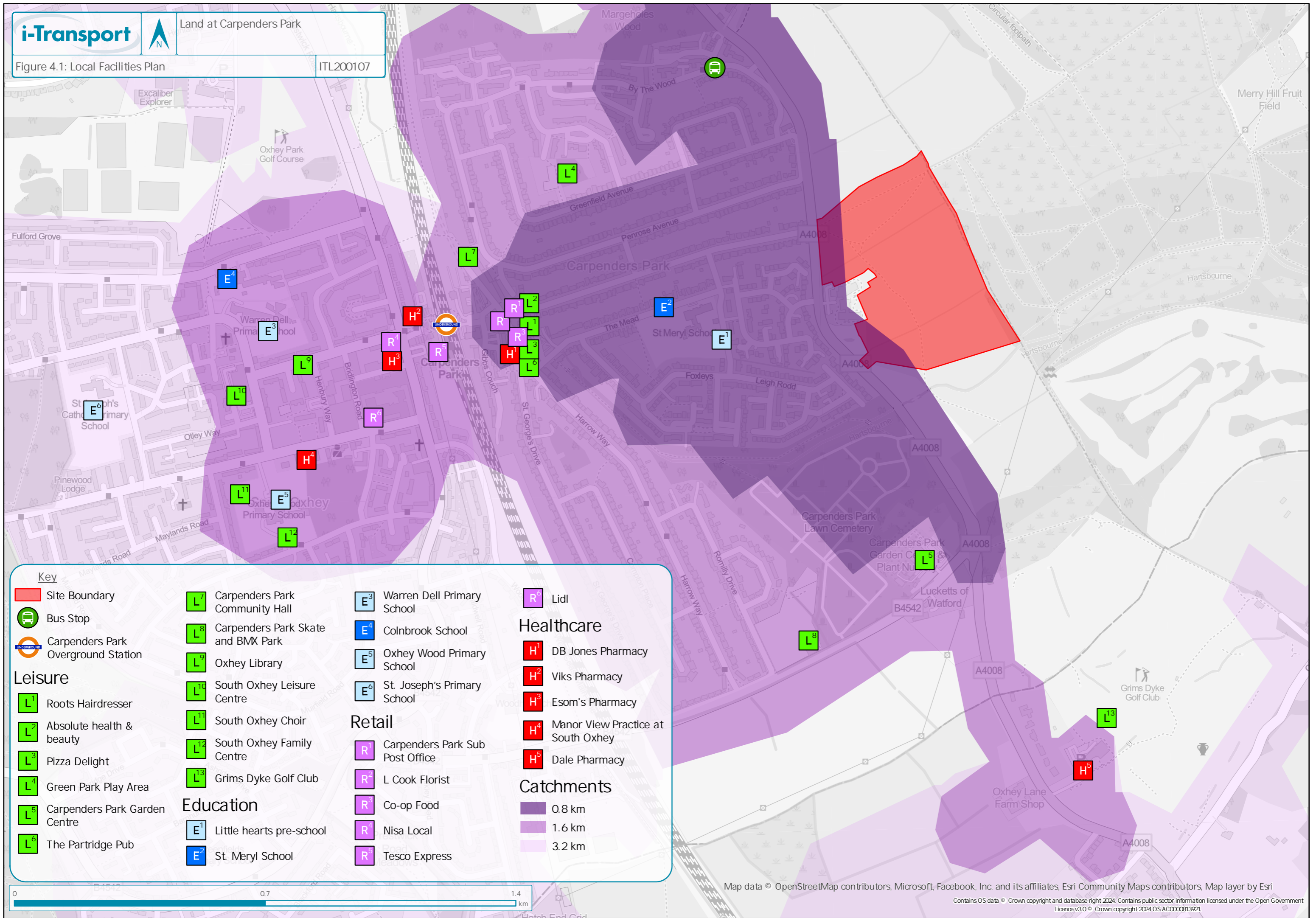
7.4.2 Accordingly, there are no transport or highways reasons for not granting planning permission.

## FIGURE 4.1



Figure 4.1: Local Facilities Plan

ITL200107



Key

- Site Boundary
- Bus Stop
- Carpenders Park Overground Station

Leisure

- Roots Hairdresser
- Absolute health & beauty
- Pizza Delight
- Green Park Play Area
- Carpenders Park Garden Centre
- The Partridge Pub

- Carpenders Park Community Hall
- Carpenders Park Skate and BMX Park
- Oxhey Library
- South Oxhey Leisure Centre
- South Oxhey Choir
- South Oxhey Family Centre
- Grims Dyke Golf Club

Education

- Little hearts pre-school
- St. Meryl School

- Warren Dell Primary School
- Colnbrook School
- Oxhey Wood Primary School
- St. Joseph's Primary School

Retail

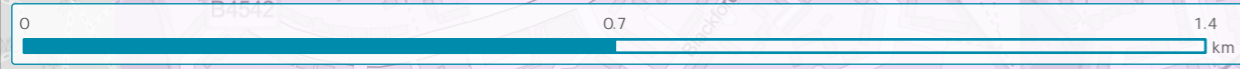
- Carpenders Park Sub Post Office
- L Cook Florist
- Co-op Food
- Nisa Local
- Tesco Express

Healthcare

- Lidl
- DB Jones Pharmacy
- Viks Pharmacy
- Esom's Pharmacy
- Manor View Practice at South Oxhey
- Dale Pharmacy

Catchments

- 0.8 km
- 1.6 km
- 3.2 km



## FIGURE 4.2



Figure 4.2: Cycling Catchment Plan

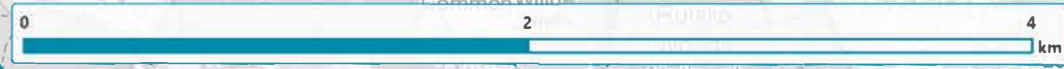
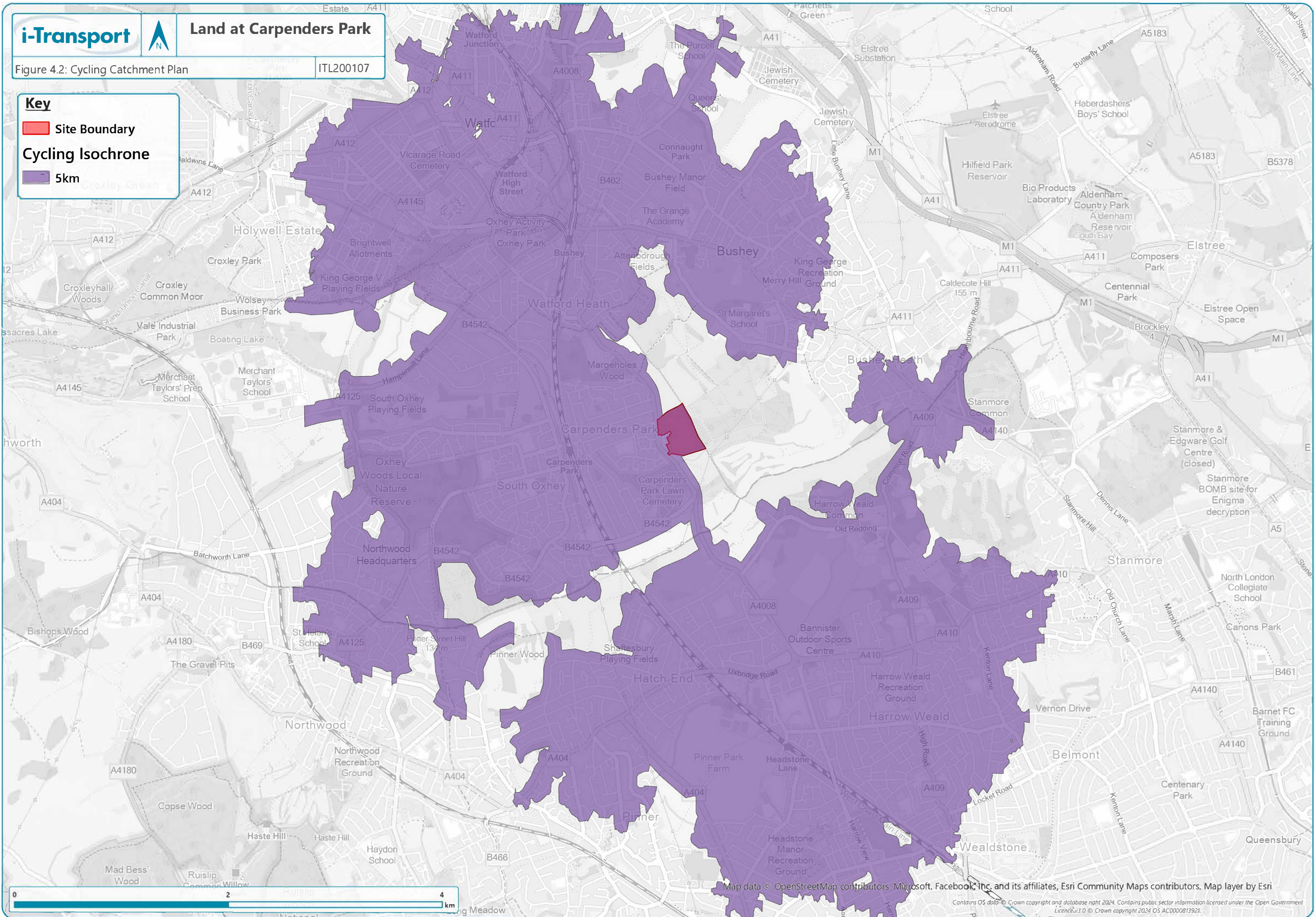
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Key

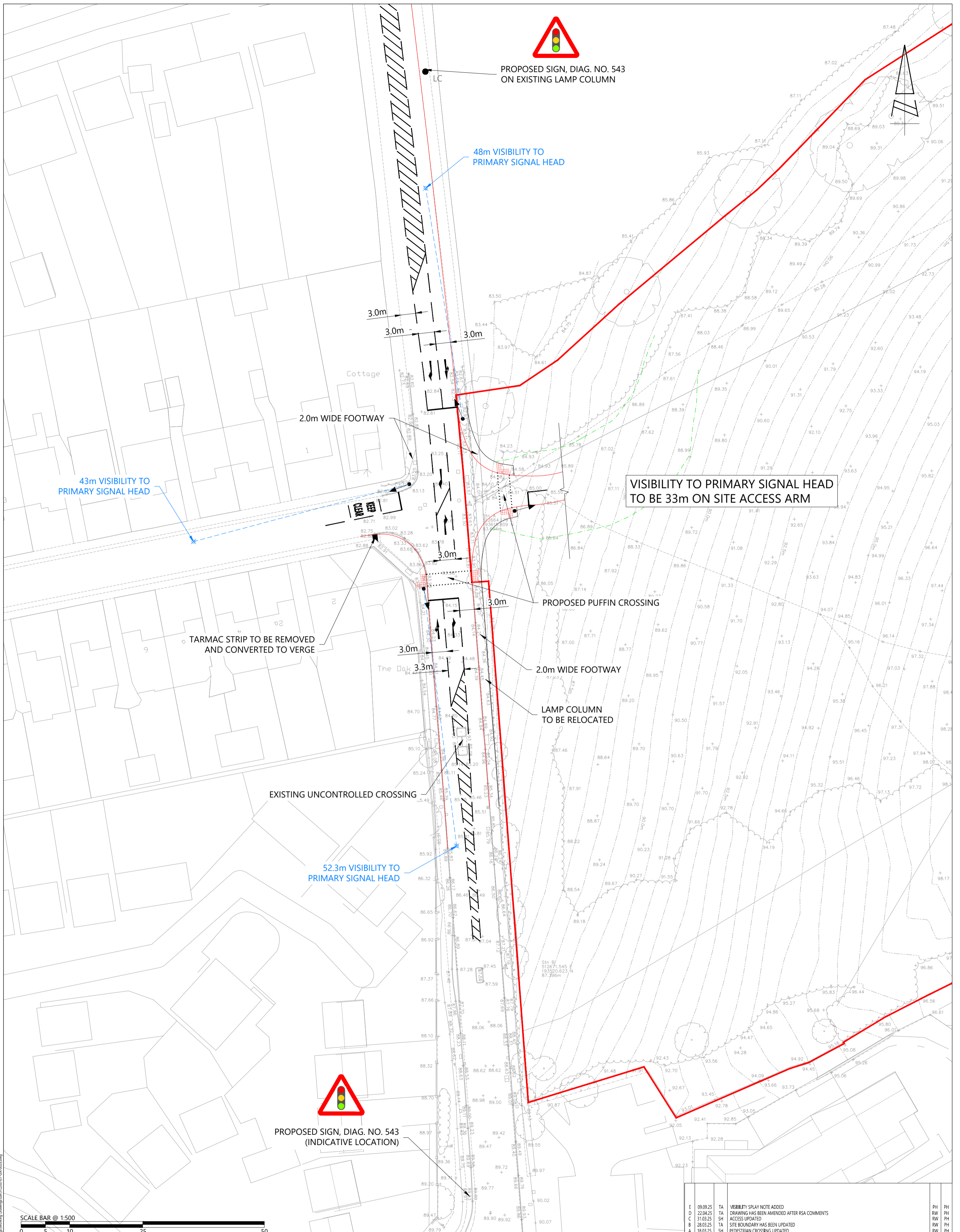
Site Boundary

Cycling Isochrone

5km



## **APPENDIX A.**



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CLIENT:  
**BURLINGTON PROPERTY GROUP**

TITLE:  
**PROPOSED SITE ACCESS ARRANGEMENT - SIGNALISED JUNCTION**

PROJECT:  
**LAND AT CARPENDERS PARK**

REV	DATE	BY	DESCRIPTION	CHK	APD
E	09.09.25	TA	VISIBILITY SPLAY NOTE ADDED	PH	PH
D	22.04.25	TA	DRAWING HAS BEEN AMENDED AFTER RSA COMMENTS	RW	PH
C	31.03.25	SH	ACCESS UPDATED	RW	PH
B	28.03.25	TA	SITE BOUNDARY HAS BEEN UPDATED	RW	PH
A	18.02.25	SH	PEDESTRIAN CROSSING UPDATED	RW	PH

STATUS:  
**FOR INFORMATION**

DRAWN:	CHECKED:	APPROVED:
SH	PH	PH

PROJECT No:	SCALE @ A2:	DATE:
ITL200107	1:500	12.11.24

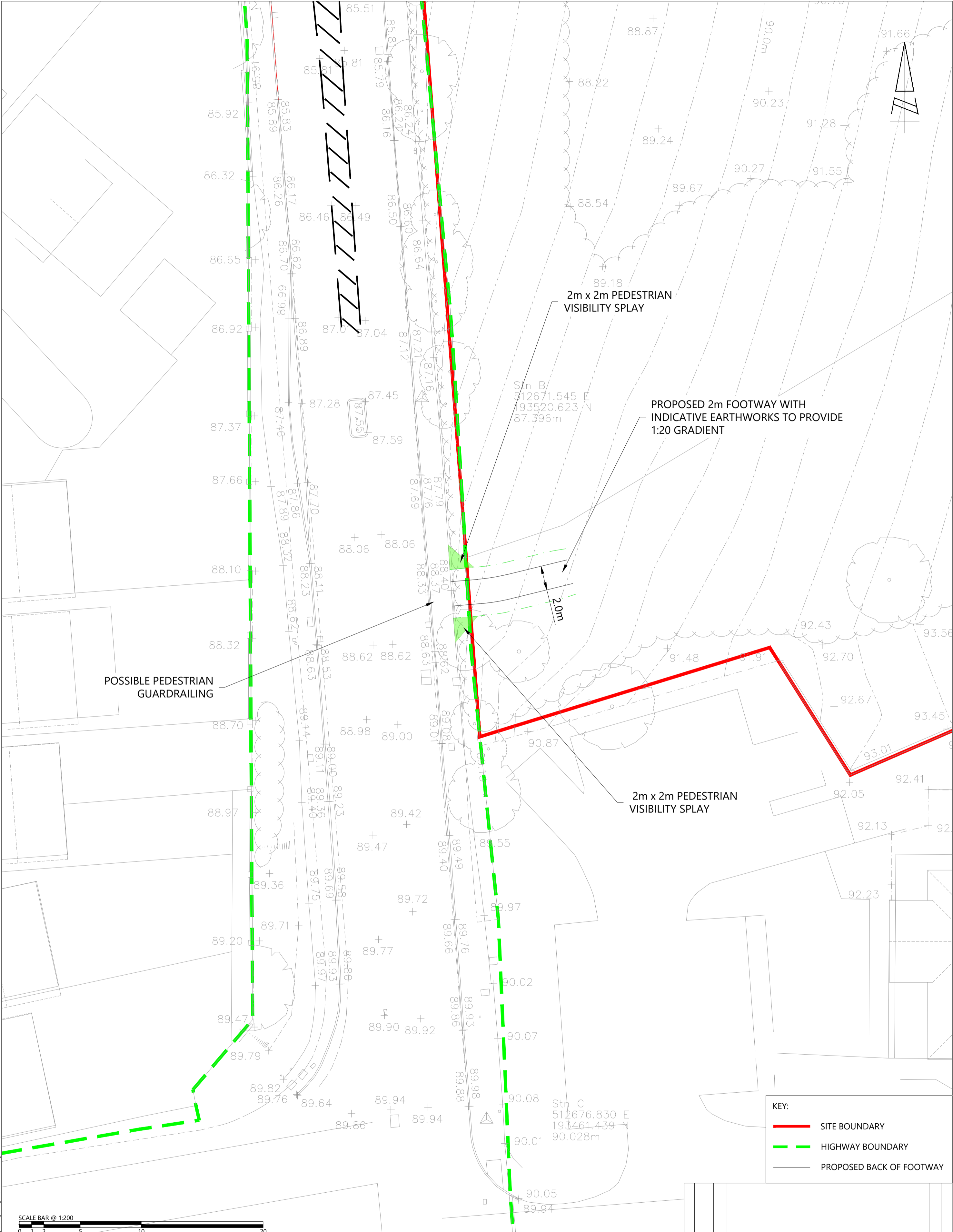
DRAWING No:	REV:
ITL200107-GA-002	E



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POSSIBLE PEDESTRIAN GUARDRAILING

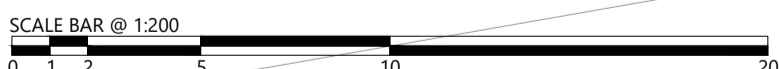
2m x 2m PEDESTRIAN VISIBILITY SPLAY

PROPOSED 2m FOOTWAY WITH INDICATIVE EARTHWORKS TO PROVIDE 1:20 GRADIENT

2m x 2m PEDESTRIAN VISIBILITY SPLAY

KEY:

<span style="color: red;">—</span>	SITE BOUNDARY
<span style="color: green;">- - -</span>	HIGHWAY BOUNDARY
<span style="color: black;">- - -</span>	PROPOSED BACK OF FOOTWAY



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CLIENT:	BURLINGTON PROPERTY GROUP					
TITLE:	PROPOSED PEDESTRIAN ACCESS ONTO OXHEY LANE					
PROJECT:	LAND AT CARPENDERS PARK					
REV	DATE	BY	DESCRIPTION	CHK	APP	
STATUS: FOR INFORMATION						
DRAWN:	TA	CHECKED:	RW	APPROVED:	PH	
PROJECT No:	ITL200107	SCALE @ A2:	1:200	DATE:	18.05.26	
DRAWING No:	ITL200107-GA-008				REV:	-

